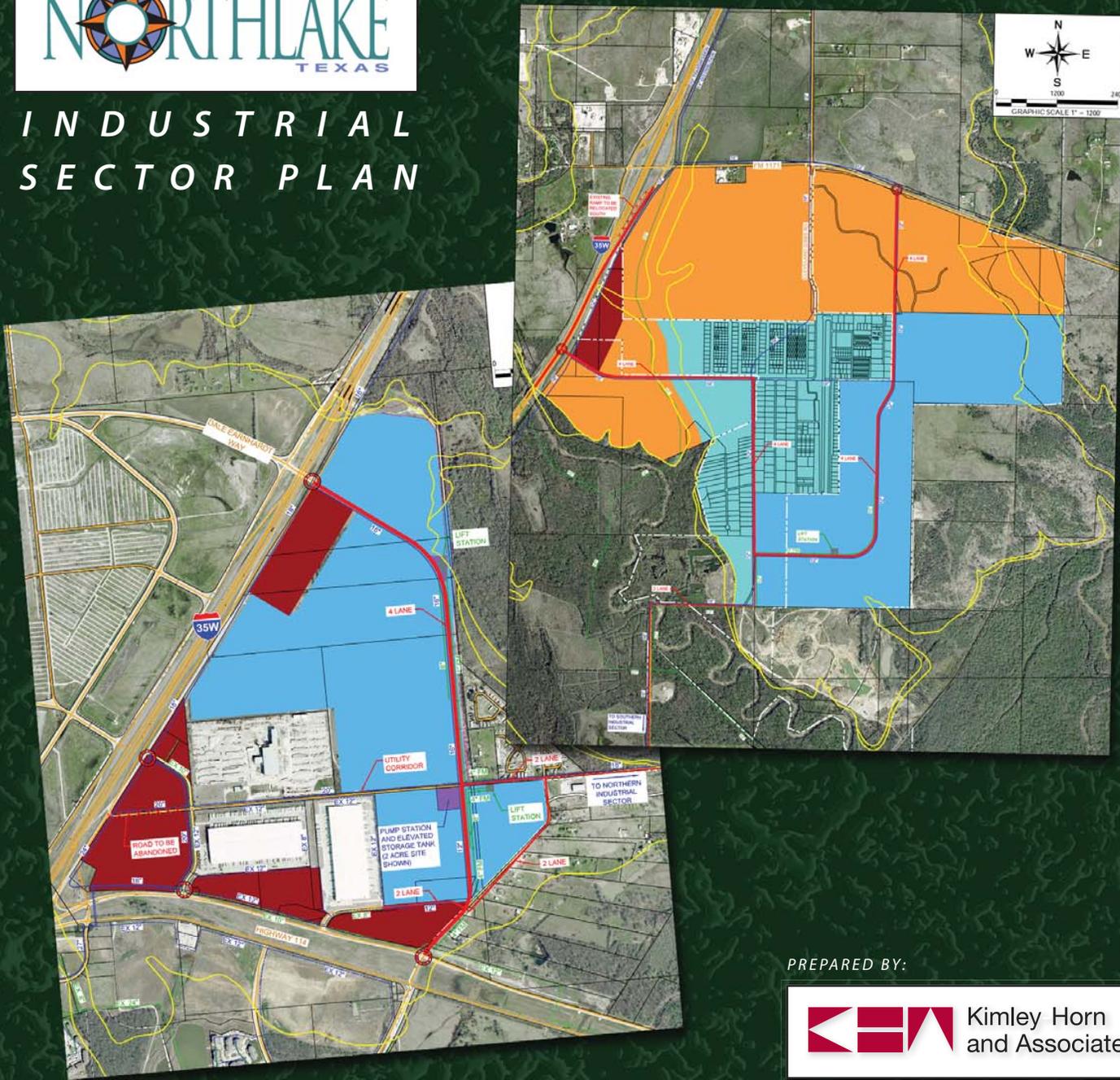




INDUSTRIAL
SECTOR PLAN

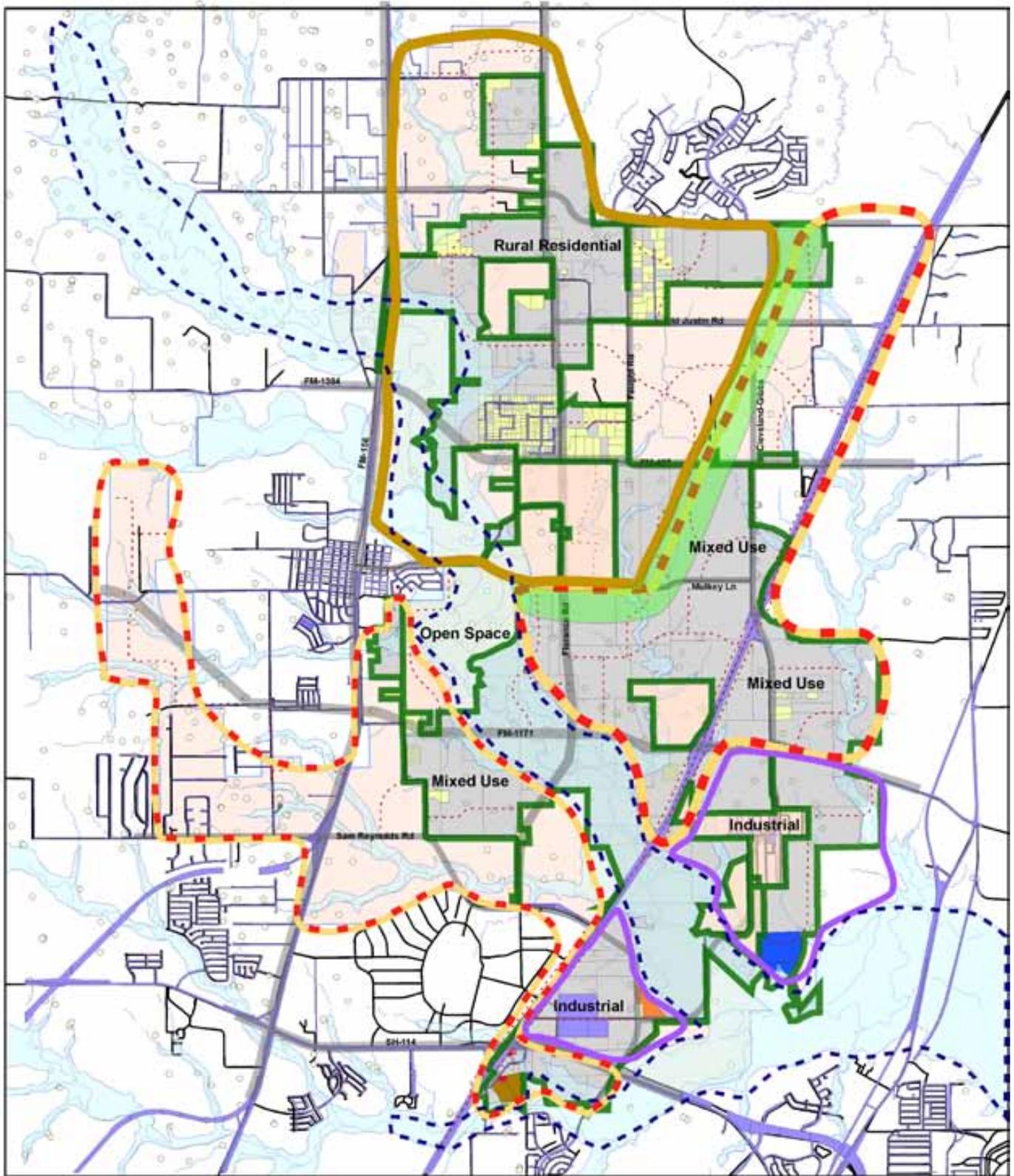


PREPARED BY:



Kimley Horn
and Associates, Inc.





Land Use Development Concept Northlake, Texas

- Rural Residential
- Mixed Use
- Open Space
- Industrial
- Transition Zone

Existing Land Use

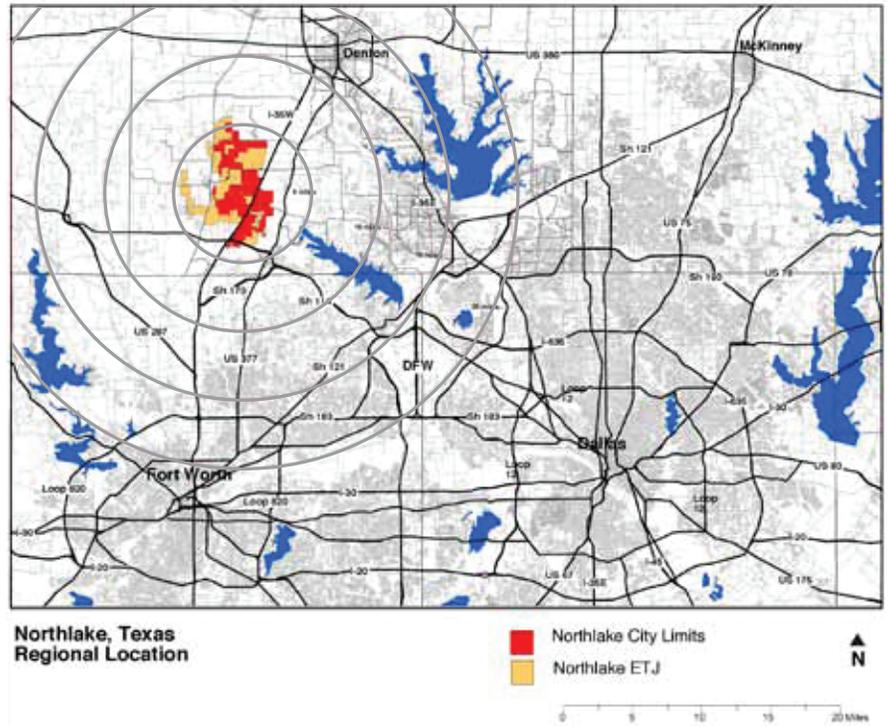
- Single Family
- Multifamily
- Mobile Home / RV
- Retail / Commercial
- Floodplain
- City Limits
- ETJ
- Industrial
- Institutional / Governmental
- Farm / Ranch / vacant
- Gas Wells

Introduction

The Northlake Vision and Comprehensive Plan, adopted in 2009, established strategic actions to guide Northlake’s future development. Several of these strategic actions targeted the town’s industrial areas with recommendations to ensure that the industrial areas were well planned and ready for quality development opportunities. Specific actions for the industrial areas included:

- Preparing strategic implementation plans to achieve or facilitate the town’s economic development goals
- Pursuing a target industry program to direct economic development efforts
- Preparing infrastructure needs assessments and long range master plans for needed infrastructure and supporting services

Several large mixed use and residential developments are occurring in the Northlake area. In addition, because of the central location, high visibility, tremendous access, low taxes, and existing high quality industrial development within the Northlake Business Center, the remaining industrial areas are starting to experience increased interest and activity.



The Town of Northlake has designated approximately 1,670 acres of land at the northeast corner of I-35W and SH 114 as industrial or supporting uses. Of these 1,670 acres, approximately 1,000 acres are anticipated to be available for industrial development. The remaining land, along the I-35W frontage is likely to develop in retail or supporting commercial or mixed uses. Northlake’s industrial area is divided by a floodplain, resulting in two separate tracts – the “North Industrial Area” and the “South Industrial Area”.

The town of Northlake currently has more than 1,100,000 square feet of industrial development along SH 114 in the South Industrial Area. The remaining land in the South Industrial Area is primarily vacant and available for development. The Northwest Regional Airport, which sits in the middle of the North Industrial Area, is surrounded by warehouses and hangers which house a variety of uses – storage, aviation-related uses, workshops and other commercial uses. In addition, a number of interim uses such as aging residential structures are scattered throughout the area.

Northlake’s industrial areas have many assets, including a prime location at the center of the region, a low tax rate, and large natural tracts of property available for development. Additionally, the area is within an established network with Denton County funding designated for improvements. The high visibility site – with easy truck ingress, egress and few conflicting adjacent uses – makes this area extremely desirable.

Opportunities and needs in the area include a need to plan for water and electricity and work around the floodplains; topographic considerations; access needs; and dealing with site constraints resulting from the airport.

The purpose of this report is to develop a consensus vision for the industrial areas which:

- Reflects the desires of property owners and stakeholders
- Determines future infrastructure needs and concept plans
- Identifies potential development opportunities; recommends design standards to ensure quality development
- And identifies implementation steps to determine next steps in ensuring that Northlake’s Industrial Areas are ready for the quality development desired by the Town and stakeholders.

Vision for the Industrial Area

Northlake’s industrial property owners have owned their land between 4 and 45 years. The primary land owners have a minimum of 120 acres with single owners holding almost 1,000 acres of land.

Stakeholders expressed consensus views on a number of critical issues. The primary consensus goal of owners and stakeholders is to retain the most flexibility possible while also providing predictability for future developers and owners. Stakeholders elaborated that there is a desire to not get boxed into a single use or type of use, but to keep options open to future innovative uses. It was agreed that the best way to achieve this flexibility is to lay out roads and utilities to maximize the use of the land and open it up for future development.



High-quality landscaping adds visual character to industrial areas.

Another key consensus goal is to ensure that development improves the town’s tax base by working to attract the best industrial development in the region. Stakeholders also recognized

that while it is important to look out over decades at the long-term market, there were also some short-term opportunities that will begin to set the character of the industrial area for years to come.

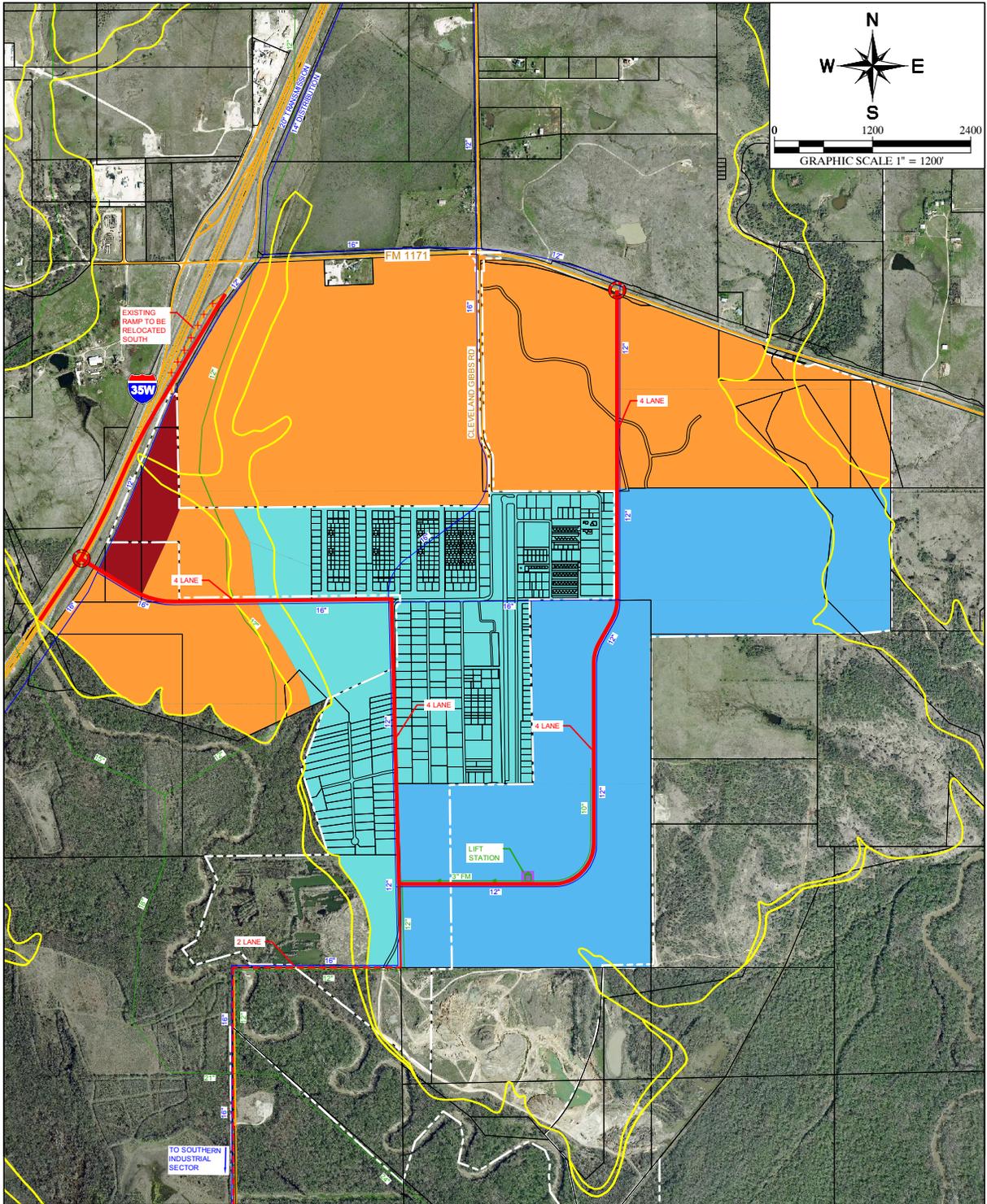
The vision for Northlake’s industrial area is one of a thriving industrial business park with a high quality look along the frontages – so that the character is consistent with the town’s rural feel. The Northlake industrial areas are envisioned to compete with other regional business parks – such as Alliance, Beechwood, Centreport, and Fossil Creek. Gateways and other design enhancements to draw focus to primary entrances and to promote the quality location are also desired.

Ultimately, stakeholders and property owners expressed their desire to develop the industrial area the “right way” with planning, forethought, and the resulting predictability to drive the type and quality of future development.

Site Plans and Site Description – Desired Development

The concept plans shown on pages 3 and 4 reflect six future land use categories and conceptual road and utility requirements. The future land use boundaries shown on these site plans are generalized and show the potential best types of development to meet Northlake’s goals:

Land Use Category	Approximate Area (Acres)	Description
Mixed Use	587	Modern mixed retail, commercial, service, and light industrial and supporting uses in a planned development environment
Retail/Commercial	125	Highway-oriented commercial and retail uses supporting surrounding industrial uses and traveling patrons
Industrial	646	Modern light industrial, distribution center, and supporting office uses; light assembly and manufacturing contained within buildings
Auxiliary Industrial	309	Region-traditional industrial and commercial uses; supporting storage functions maintained with screening and use controls
Public	3	Municipal use; facilities and services
Total	1670	



LEGEND

PARCEL BOUNDARY	
EXISTING WATER	
EXISTING SEWER	
PROPOSED WATER	
PROPOSED SEWER	
PROPOSED FORCE MAIN	
PROPOSED ROAD	
PROPOSED ROAD OPTION	
EXISTING ROAD	
GAS EASEMENT LIMITS	
FLOODPLAIN LIMITS	
TOWN LIMITS	
RETAIL/COMMERCIAL USE	
INDUSTRIAL USE	
AUXILIARY INDUSTRIAL	
PUBLIC USE	
MIXED USE	
PRIMARY ACCESS POINT	



NORTHERN INDUSTRIAL SECTOR PLAN

NOTE

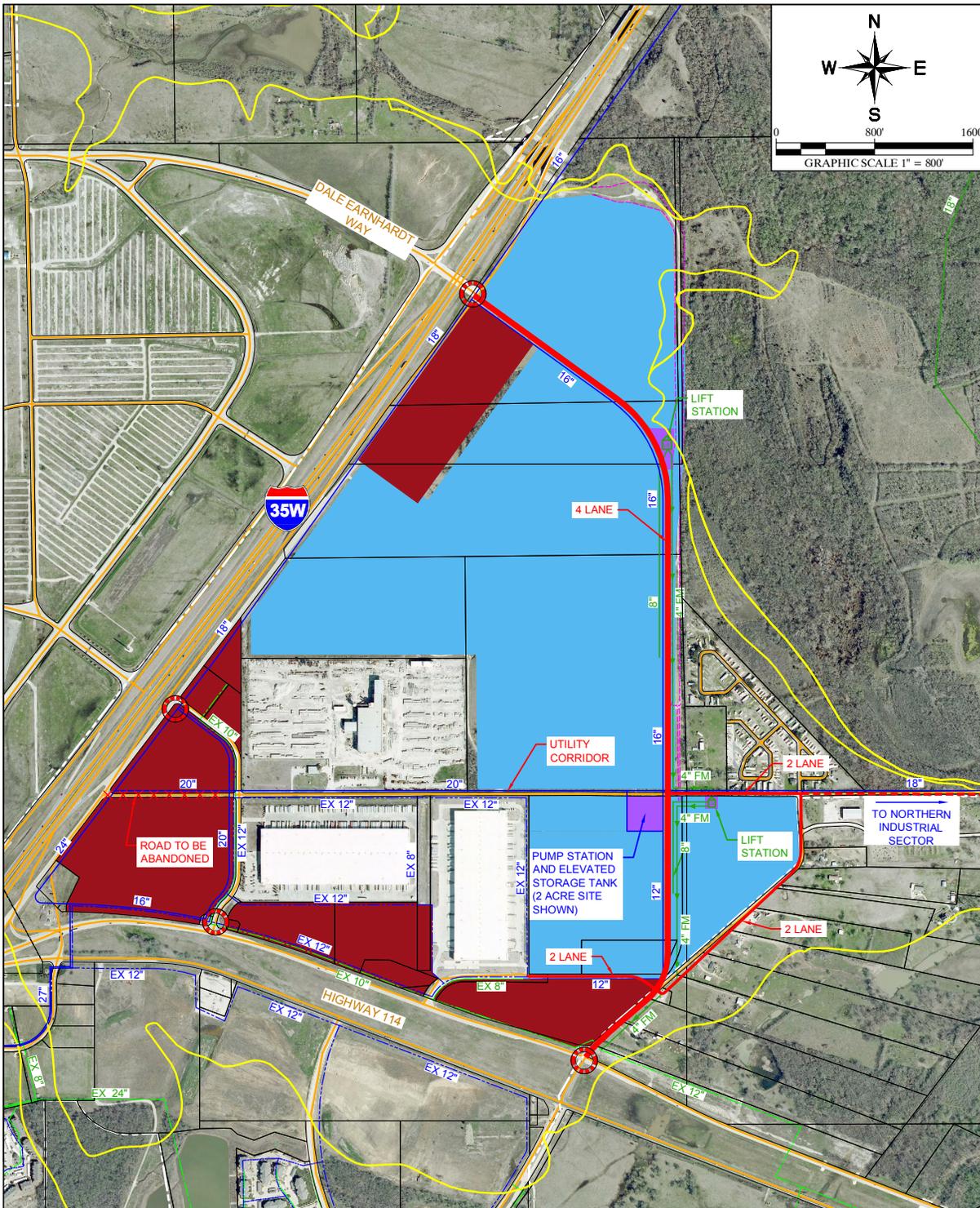
1. ALL SITES AND ALIGNMENTS ARE CONCEPTUAL AND SUBJECT TO FURTHER STUDY AND CHANGE UNTIL ADOPTION BY THE TOWN.
 2. PROPOSED WATER LINE SIZES BASED ON PRELIMINARY WATER MODEL PROVIDED BY HALFF
- JANUARY 12, 2012



Kimley-Horn and Associates, Inc.

12700 Park Central Drive, Suite 1800
Dallas, Texas 75251
972-770-1300

Scale 1" = 1200'	Drawn by DPG	Checked by DFG	Date 1/24/2012	Project No. 064446600	Sheet No. 1 OF 1
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LEGEND

PARCEL BOUNDARY	---
EXISTING WATER	— · — · — ·
EXISTING SEWER	— · — · — ·
PROPOSED WATER	— · — · — ·
PROPOSED SEWER	— · — · — ·
PROPOSED FORCE MAIN	— · — · — ·
PROPOSED ROAD	— · — · — ·
PROPOSED ROAD OPTION	— · — · — ·
EXISTING ROAD	— · — · — ·
GAS EASEMENT LIMITS	— · — · — ·
FLOODPLAIN LIMITS	— · — · — ·
TOWN LIMITS	— · — · — ·
RETAIL/COMMERCIAL USE	— · — · — ·
INDUSTRIAL USE	— · — · — ·
AUXILIARY INDUSTRIAL	— · — · — ·
PUBLIC USE	— · — · — ·
MIXED USE	— · — · — ·
PRIMARY ACCESS POINT	⊙



SOUTHERN INDUSTRIAL SECTOR PLAN

NOTE

1. ALL SITES AND ALIGNMENTS ARE CONCEPTUAL AND SUBJECT TO FURTHER STUDY AND CHANGE UNTIL ADOPTION BY THE TOWN.
 2. PROPOSED WATER LINE SIZES BASED ON PRELIMINARY WATER MODEL PROVIDED BY HALFF
- JANUARY 12, 2012



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Scale 1" = 800'	Drawn by DPG	Checked by DFG	Date 1/24/2012	Project No. 064446600	Sheet No. 1 OF 1
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Public Improvements Descriptions

Infrastructure Planning

The conceptual infrastructure shown on the concept plans is based on considerable experience with industrial parks and proposed industrial-related land uses. The intent of the infrastructure elements shown on the concept plans is to identify issues and guide further study and planning of area infrastructure. It is not intended to set final infrastructure alignments and sizing.

It was anticipated and discussed during development of the plan that the pattern of development and actual uses will determine final alignments and sizing, while following the plan presented in this document. Additionally, these conceptual plans provide guidance to the Town for developing capital improvement program projects to coordinate with the needs of private development.

Ultimately, the goal of future improvements requires meshing private development and public infrastructure for mutual benefit and sustainable economic development. Recommendations for infrastructure considerations critical to the types of land uses projected for Northlake's industrial areas include:

- Roadways designed in cross section and pavement type for anticipated traffic, including in almost all cases heavy truck volumes
- Water distribution capable of delivering fire flow volume and rate necessary to exceed high fire loss insurance requirements often placed on industrial distribution development
- Proper ratio of ground and elevated water storage availability to meet typical lower daily demand and higher fire flow demand of industrial development
- Application of realistic sanitary sewer flow rates when sizing and planning infrastructure; this infrastructure is often oversized when sized using only published demand rates
- Phasing of infrastructure so to anticipate future development without creating excessive financial burden on the pending development or the Town
- Consideration of stormwater detention on a development-by-development basis, given the location and conveyance to an adequate outfall; ultimately Denton Creek and Lake Grapevine

Continued planning and design consultation is required to ensure that the stakeholder's vision is carried through the infrastructure engineering design and construction process.

Design Considerations and Potential Guidelines

Design guidelines for industrial development ensure that consistent high quality sustainable development occurs to protect private and public sector investments. The goal of the industrial design guidelines is to promote the following principles which ultimately bring predictability to Northlake's industrial development:

- Development patterns promote a functional and attractive environment.
- Buildings exhibit a "corporate" architectural character of high quality materials, design, and color. Development is designed for overall cohesiveness and respects local natural features.
- Landscaping and entryway features are used to clearly identify the Northlake industrial area and to provide a cohesive business park atmosphere.
- Buildings are oriented so that loading, storage, and other external activities and building features that generate noise, etc., are not in view of public rights-of-way. Loading and outdoor storage and industrial activities are screened from public views.
- Private property values are protected and enhanced.
- Public investments are protected.
- The development approval process is expedited and quality business park projects are facilitated, resulting in positive economic development activities.
- Public utilities and services are provided in an efficient and expeditious manner.

Goals for Specific Areas:

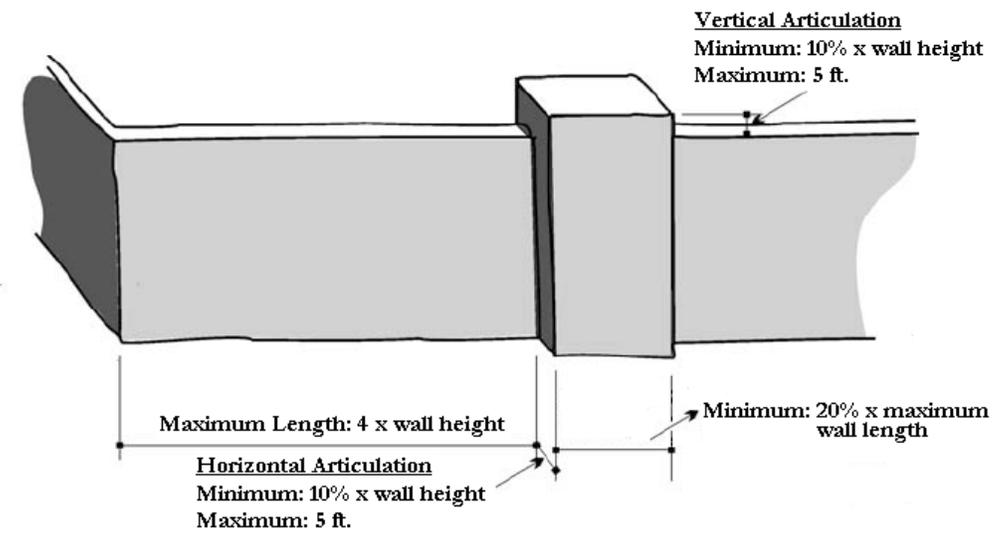
Industrial Park Edge (along SH 114 and I-35W): Create immediate first impressions of a high quality industrial park for surrounding land uses, owners and visitors of the companies located in the park.

Industrial Park Gateways: Gateway identity features appropriate in image and style of the park and surrounded by foundation planting should be located at primary entryways into the industrial park to promote image of cohesive development.

Existing Industrial Regulations:

	Existing Standards	Recommended Standards
Front Setback	20 feet	Retain 20 feet
Side Setback	None or 10 feet	Retain None or 10 feet
Rear Setback	10 feet	Retain 10 feet
Maximum height	35 feet	Change to 65 feet
Exterior Materials	<p>The exterior of any primary building shall be constructed of at least eighty-five percent (85%) masonry material, not including doors and windows. Upon approval by the Town Council, the percentage of masonry materials for secondary buildings may be reduced. Masonry material shall be defined as that form of construction composed of brick, stone, concrete, gypsum, hollow-clay tile, glass block, or similar building materials or combination of these materials laid up unit by unit and set in mortar. (Masonry does not include stucco, exterior insulated finish systems (EIFS), or “Hardiplank/ Hardipanel” or other similar sheet masonry products.) Metal buildings used as a main structure without a masonry or wood exterior surface shall be prohibited. Allow articulated metal buildings in area around airport – clearly define standards</p>	<p>Change to: Combination of materials from an approved list may be used on 100% of exterior wall surfaces. These may include stone, brick, glass block, tile, cast metal, cast or cultured stone, and concrete (tilt-up walls). Cement products (stucco, Hardy Plank, etc.) should be limited to 50% of surfaces if appropriately designed and applied. Allow exceptions on a case by case basis.</p> <p>Allow architecturally enhanced metal buildings with articulated fronts and roofs in transition areas, such as the Northern Sector Plan Area designated as “Auxiliary Industrial”. Clearly define standards and location.</p>
Landscaping	<p>In the Industrial zoning district, only the front-yard forty percent (40%) of the total twenty percent (20%) of landscaping requirement shall be required. The rear and side yard landscape requirements may be waived upon submittal of a landscape plan showing other requirements.</p> <p>(Additional requirements per landscape ordinance.)</p>	Retain existing landscaping standards.

New Standards for Industrial Areas

Topic	Proposed Standard
<p>Articulation</p>	<p>Avoid long, monotonous facades with variation in roof lines and changes in wall plane. Require variation in horizontal and vertical off-sets of walls facing public right-of-ways and residential areas.</p> <p>Example:</p> <p>Horizontal Articulation. Walls facing a public right-of-way or a residentially zoned property shall not extend for a distance greater than four times the wall's height without having an off-set of ten percent of the wall's height (maximum of five feet); the new plane shall extend for a distance equal to a minimum of twenty percent of the maximum length of the first plane.</p> <p>Walls not facing a public right-of-way or a residentially zoned property and loading dock doors are exempt from the horizontal articulation requirement.</p> <p>Vertical Articulation. Walls facing a public right-of-way or a residentially zoned property shall not extend for a distance greater than four times the height of the wall without changing height by a minimum of ten percent of the wall's height (maximum of five feet).</p> <p>Walls not facing a public right-of-way or residentially zoned properties are exempt from the vertical articulation requirement.</p> 
<p>Front elevation landscaping</p>	<p>Specify landscaping along front elevations or elevations that face public streets. Exclude dock areas.</p> 

Topic	Proposed Standard
<p>Earth-toned colors</p>	<p>Require at least eighty percent of each façade be neutral, crème, or deep, rich, non-reflective natural or earth-toned colors, and no more than one color may be used for visible roof surfaces.</p> <div style="display: flex; justify-content: space-around;">   </div>
<p>EIFS</p>	<p>The use of exterior insulated finishing system (EIFS) is not allowed below ten (10') feet above finished grade.</p>
<p>Glass</p>	<p>Total window area should not exceed fifty percent of street-facing façades.</p>
<p>Open Storage</p>	<p>Limit open storage areas to accessory uses to a main use located in a building.</p> <p>Prohibit open storage in front of the main building</p> <p>Prohibit wrecking, junk or salvage yards.</p>
<p>Mechanical unit screening</p>	<p>Screen all mechanical equipment from adjacent public streets and residential areas. Screening must match building color and material. Ground-mounted mechanical units may be screened with an evergreen landscape screen. Measured from 5' above property line.</p>
<p>Rooftop Equipment Screening</p>	<p>All rooftop mounted mechanical, air conditioning, electrical, and satellite dish equipment shall be completely screened from ground and street level view with parapets or other architectural design features constructed of the same materials used on the exterior walls. Measured from 5' above the property line.</p>
<p>Exposed utilities</p>	<p>Paint exposed conduit, ladders, utility boxes, and drain spouts to match the color of the building or an accent color.</p>
<p>Trash and recycling collection areas</p>	<p>Locate trash and recycling collection areas to minimize visibility. Screen trash receptacles, recycling receptacles, and trash compactors with a wall of a consistent color and material as the primary building. Orient enclosures so that the service opening does not face a public right-of-way.</p> <p>Dumpster and Compactor Screening. All dumpsters and compactors visible from public right-of-way and/or abutting residential, commercial, public, or civic property shall be screened with a consistent six foot opaque screening wall of a consistent color and material as the primary building. Provide a solid metal gate. Chain link fences or wooden fences are not permitted. Dumpsters shall be set back a minimum of twenty-five feet from adjacent residential uses.</p> <div style="text-align: right;">  </div>

Topic	Proposed Standard
<p>Loading areas</p>	<p>Screen loading and service areas from view from adjacent public streets and adjacent residential areas. Screen cross-docks from view along industrial park boundaries (such as the areas along I-35W, SH 114 and FM 1171).</p> 
<p>Lighting</p>	<p>Require pedestrian lighting (lower level lighting) to illuminate pedestrian walkways that generally run parallel to a street or traverse a parking lot along a clearly demarcated walkway.</p>
<p>Building entries</p>	<p>Locate building entries so they are easily identifiable with convenient public access.</p> 
<p>Monument Entranceway signs</p>	<p>Permanent signs relating to the Northlake Industrial area as a whole shall be permitted with a maximum size of forty square feet. All signs shall be constructed using stone, stucco, brick, tile, or similar materials.</p>

Implementation Plan – Next Steps

During development of the Industrial Plan, a number of next steps were discussed. Those that can be implemented in advance of actual private development projects to further the Town’s vision for the industrial area are listed below. A brief description of scope and approximate fee (if necessary) accompanies each plan step. The steps are in no particular order, but should be undertaken as soon as possible to build upon the momentum the Industrial Plan study has created.



Entryway features clearly identifiable access points.

Task	Description	Estimated Cost and Schedule
Stormwater Detention Timing Study	Hydrologic Study of the Denton Creek watershed with the influence of potential stormwater timing within the industrial area. The objective of the study is to support the requirement for development to detain stormwater to existing site conditions, or to only provide for conveyance of the increased volume and flow rate to the nearest location with adequate capacity.	Approximately \$25,000 for study of the entire Industrial Plan area; three months to complete.
Amendment of relevant ordinances	Town ordinances affected by the policy adopted in this Industrial Plan should be amended for consistency and enforceability. Ordinance amendments should include at a minimum adjustments to the land use plan and inclusion of enhanced industrial design standards into the current zoning ordinance rewrite process.	Include in current ordinance revision project.
Entranceway Features	Property owners should work with town staff to develop consistent high quality entranceway features at each of the entranceways designated on the Site Plans. These features will establish the quality of the future business parks and promote the entire area for future development and investment.	Cost is contingent on feature design. Potential funding for the construction and maintenance of these amenities could be through private agreements or implementation of a Public Improvement District.
Consideration of Street Parking Ordinance	Town Staff should gather public and technical input concerning the parking of trucks in public rights-of-way for extended periods or overnight. Implementation of a truck parking ordinance provides a law enforcement tool should this nuisance typical to primary thoroughfares in industrial parks becomes an issue for the Town.	Include in current ordinance revision project.

Task	Description	Estimated Cost and Schedule
<p>Northbound I-35W Exit Ramp to FM 1171 Relocation</p>	<p>There are a number of steps required to advance this major infrastructure improvement. The relocation of the ramp south to extend the northbound frontage road and create access to the planned east-west thoroughfare will be key to the successful and sustainable development of the northern industrial area. This project will be required to follow TxDOT's Project Development Process, and will require partnership between the Town and the State. The Town can instigate the process by taking two initial steps:</p> <ol style="list-style-type: none"> 1. Adopt the East-West Thoroughfare in the northern industrial area on the Town's Master Thoroughfare Plan. This justifies a 'need' for access from a public point of view. 2. Sponsor the development of a Preliminary Schematic and Opinion of Probable Construction Cost (OPCC) for the ramp relocation and frontage road extension. This engineering layout would serve as the Town's 'request' to the State of Northlake's desire for the project. Many steps beyond the Preliminary Schematic and OPCC are required, involving the Town, TxDOT, and the North Central Texas Council of Governments. Funding will be of prime concern to TxDOT as well. 	<p>Approximate fee to develop this document and conduct coordination within TxDOT is \$40,000.</p>
<p>Industrial Marketing Brochure</p>	<p>Development of a Town-sponsored brochure capturing the results of the Industrial Plan for the Town's use in promoting private development.</p>	
<p>Awareness of Public Projects</p>	<p>This activity may be 'business as usual' for the Town, but is worth noting. Staying abreast of planned public improvements, primarily TxDOT roadway and Fort Worth Water, and their impact and effect on this will help the Town realize their vision for the area. Interested private development will inquire about these items to learn of potential hurdles to development success.</p>	